

4 ● COMMUNITY MOBILITY



Introduction

As with every community, the ability to move efficiently from place to place is essential to overall economic health and vitality. As a major employment center, priorities for Greater Airpark mobility include ease of access, traffic flow, and adequate parking. Successful employment centers are those where land uses are conveniently connected to each other, easily accessed by major thoroughfares, and, above all, focus on providing a high-quality, convenient transit system.

The Greater Scottsdale Airpark is not only a destination for employees, but visitors, shoppers, and local residents as well. This diversity of travelers requires a complete, integrated circulation network connecting local and regional destinations. A successful circulation system will fulfill the needs of pedestrians and bicyclists, make transit a highly desirable and efficient mode of travel, continue to provide excellent access to and from the Loop 101 Freeway system, and provide appropriate parking solutions for all types of visitors. Transit is among the highest of priorities to accomplish the goals and policies set forth in the Greater Airpark Character Area Plan.

As with many other components of the Greater Airpark Character Area Plan, collaboration among the City, businesses, employees, property owners, and other Greater Airpark community members is essential to the successful implementation of this element.

Goals and Policies

GOAL CM 1

Strengthen transit in the Greater Airpark as the primary means of reducing vehicular traffic congestion, minimizing parking constraints, promoting environmental stewardship, and, as a result, improving regional air quality.

- **Policy CM 1.1**
Enhance Greater Airpark connections to local and regional public transportation systems.
- **Policy CM 1.2**
Promote common parking areas connected to public transit that will serve employees, residents, and visitors to the Greater Airpark and surrounding areas.
- **Policy CM 1.3**
Enhance local bus route service frequency and hours of operation to better serve the employment core and tourist destinations.
- **Policy CM 1.4**
Integrate additional transit routes in areas identified as Signature Corridors, to serve their associated mix of experiences, and other streets in the Greater Airpark, as needed.
- **Policy CM 1.5**
Support the development of a Greater Airpark circulator and transit centers, through public and private partnerships.
- **Policy CM 1.6**
Improve transit access from Loop 101 High Occupancy Vehicle (HOV) lanes to the Greater Airpark.
- **Policy CM 1.7**
Encourage companies to provide “after hours” transportation, and other guaranteed ride-home programs, for employees who commute by using modes other than single-occupant vehicles.



Circulators, like the Scottsdale Trolley (pictured), can help move people around the Greater Airpark efficiently and comfortably.



Bus route wait time displays make transit use more convenient and user-friendly.
Photo source: Nextbus.



Transit facility designs should provide air circulation and shading.

- **Policy CM 1.8**

Incorporate Scottsdale Sensitive Design Principles, public art, adequate shading, proper air circulation and ventilation, seating orientation, and other climactic comforts into the design of transit facilities to enhance transit-user experiences and overall Greater Airpark aesthetics.

- **Policy CM 1.9**

Ensure transit-user safety, convenience, and comfort in the design of Greater Airpark transit facilities by using, for example, adequate lighting, universal accessibility, transit route information, and time displays.

- **Policy CM 1.10**

Support and achieve Greater Airpark Character Area Plan mobility goals through establishment of a Greater Airpark Transportation Management Authority (TMA) with the intent to reduce travel and parking demand in the area.

GOAL CM 2

Holistically manage the supply of existing and future parking in the Greater Airpark.

- **Policy CM 2.1**

Continue to promote shared-parking in all existing and future developments.

- **Policy CM 2.2**

Create parking management districts served by transit in the Greater Airpark to effectively manage the supply of parking in the area, particularly in mixed-use areas.

- **Policy CM 2.3**

Provide preferred parking facilities for bicycles, carpool vehicles, vanpool vehicles, and motorcycles.

- **Policy CM 2.4**

Partner with area businesses and property owners to accommodate overflow parking to serve special events and regional attractions, such as WestWorld and the Tournament Players Club Golf Course.

GOAL CM 3

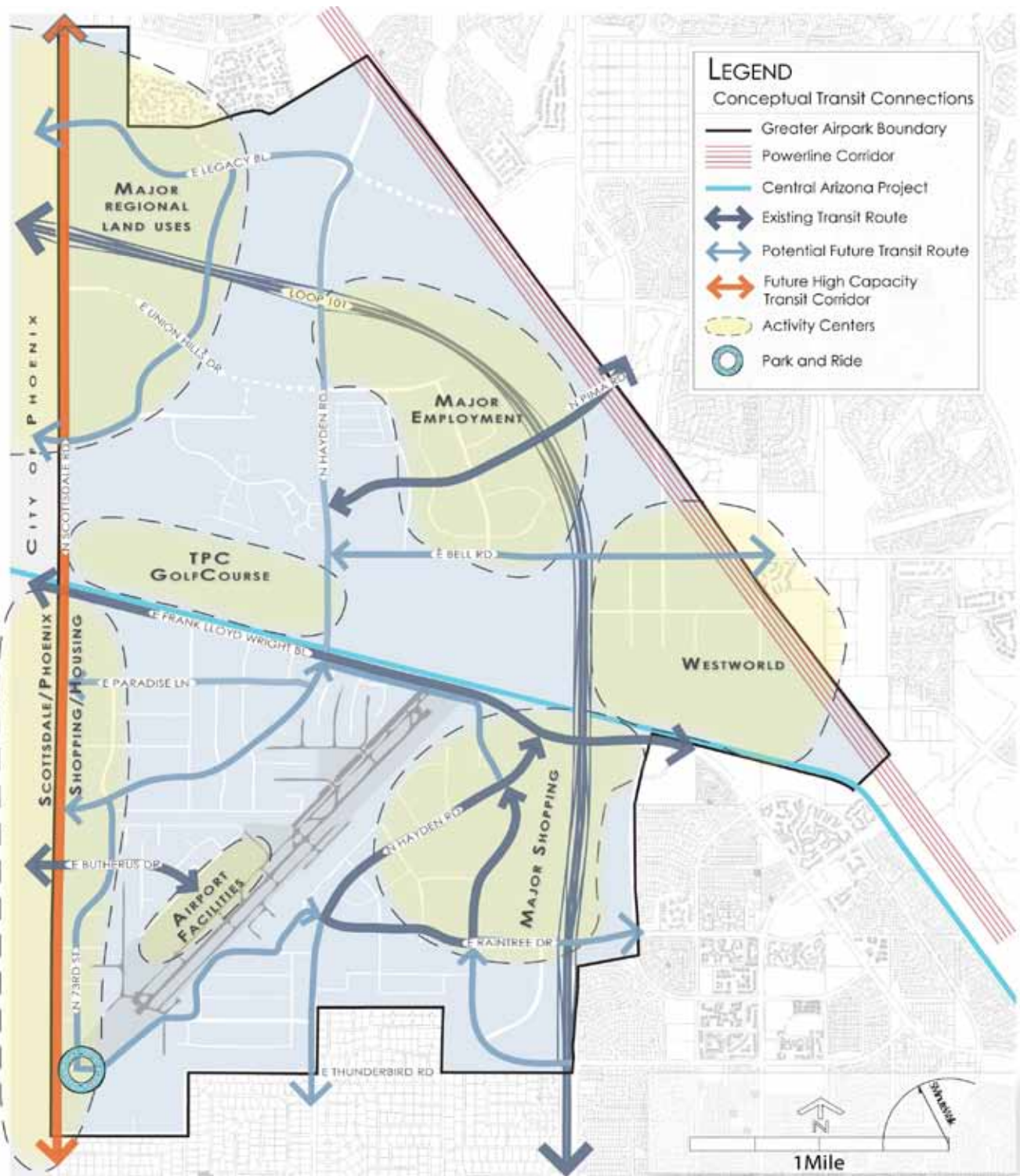
Improve vehicular traffic circulation in the Greater Airpark.

- **Policy CM 3.1**

Encourage capacity and operational improvements on streets to improve circulation, where needed, per the Transportation Master Plan policies for capacity improvements.

- **Policy CM 3.2**

Improve vehicular accessibility from the Loop 101 to the Greater Airpark.



This map presents a potential future development vision and does not constitute regulatory standards. All projects are subject to development review.

GREATER AIRPARK TRANSIT CONNECTIONS

This map illustrates areas where transit connections exist, as well as, potential locations for future transit routes and transit centers in the Greater Airpark.

- **Policy CM 3.3**
Develop predictable, convenient wayfinding systems for visitors.
- **Policy CM 3.4**
Encourage staggered delivery times and shared delivery areas throughout the Greater Airpark to reduce street obstructions.
- **Policy CM 3.5**
Encourage employee telecommuting, staggered work hours, flextime, and company car/van-pooling to reduce overall vehicle miles traveled.

GOAL CM 4

Minimize the impacts of Greater Airpark vehicular traffic on adjacent residential neighborhoods.

- **Policy CM 4.1**
Design Greater Airpark streets that discourage cut-through traffic into neighboring residential areas.
- **Policy CM 4.2**
Improve pedestrian and bicycle connections from adjacent neighborhoods to Greater Airpark destinations.
- **Policy CM 4.3**
Preserve Thunderbird Road, west of Loop 101, and other neighborhood streets immediately south of the Greater Airpark, as neighborhood-serving streets.



Streets south of the Greater Airpark, such as Thunderbird Road and Sweetwater Drive (pictured), should remain neighborhood-serving.

GOAL CM 5

Maintain and enhance use of the Greater Airpark trail and path system.

- **Policy CM 5.1**
Encourage equestrians and other nonmotorized travelers to utilize the area's non-paved trail systems.
- **Policy CM 5.2**
Connect existing and future trails and paths to city-wide and regional systems.
- **Policy CM 5.3**
Connect the McDowell-Sonoran Preserve to the Greater Airpark through trails and a recognizable wayfinding system.
- **Policy CM 5.4**
Incorporate trails and pedestrian underpasses into the power line corridor to maximize its use.



Equestrians are encouraged to utilize the area's non-paved trail system.

GOAL CM 6

Enhance pedestrian and bicyclist access and activity for Greater Airpark residents, visitors, and employees.

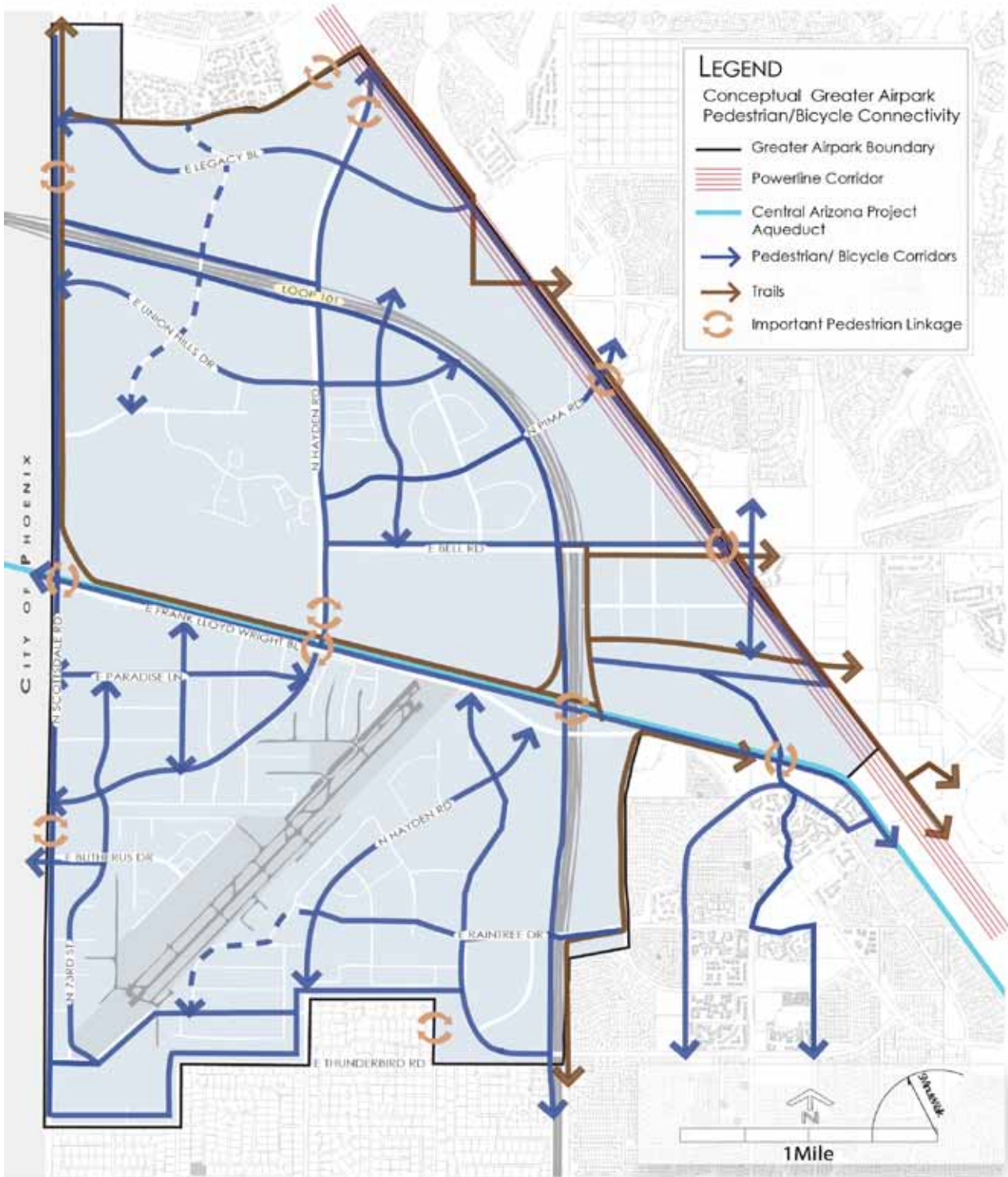
- **Policy CM 6.1**
Provide mid-block pedestrian and bicycle connections throughout the Greater Airpark.
- **Policy CM 6.2**
Support an attractive, safe, and engaging pedestrian and bicyclist environment for all users.
- **Policy CM 6.3**
Encourage innovative local and regional multi-modal connections between public and private destinations and attractions.
- **Policy CM 6.4**
Partner with various governmental agencies to create, enhance, and expand multi-modal connections among different jurisdictions and agency owners.
- **Policy CM 6.5**
Design corridors that accommodate and attract pedestrians and bicyclists, particularly in Airpark Mixed Use Future Land Use Areas and along Signature Corridors.
- **Policy CM 6.6**
Design safe, comfortable, and aesthetically-pleasing Greater Airpark pedestrian and bicyclist facilities through the incorporation of universally accessible designs, coordinated street lighting, visually-interesting landscape treatments, shading, bicycle lanes, and public art integrated into facility design.
- **Policy CM 6.7**
Enhance connections across or around barriers, such as the Central Arizona Project Aqueduct, Loop 101 Freeway, and Scottsdale Airport.



Bicycle facilities, such as bicycle lanes and crossing signals, promote better bicycle access and circulation.



Signals, cross-walks, and/or pedestrian refuges or other crossings are needed for safety and convenience where destinations are bisected by major streets, such as Scottsdale Road, which separates Kierland Commons and Scottsdale Quarter.



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GREATER AIRPARK PEDESTRIAN/ BICYCLE NETWORK

Pedestrian/ bicycle corridors, trails, and important pedestrian linkages, as shown above, promote easier access for nonmotorized methods of transportation.

GOAL CM 7

Promote sustainable transportation options that meet the needs of the current and future Greater Airpark community.

- **Policy CM 7.1**

Incorporate site design features that promote more access to those walking, cycling, or taking public transit, such as ground-floor retail and parking located in the rear of buildings, particularly along Signature Corridors and within Airpark Mixed Use Future Land Use Areas.

- **Policy CM 7.2**

Promote more sustainable modes of passenger transportation, such as alternative fuel vehicles, walking, biking, and/or other future technologies.

- **Policy CM 7.3**

Incorporate environmentally sensitive materials and technologies in transportation projects, infrastructure improvements, and facilities, including the use of solar technology and recycled materials.

- **Policy CM 7.4**

Explore and incentivize alternative mobility options, such as car sharing and shuttle buses, to access internal, as well as external, areas where public transit is less frequent and/or inaccessible.

- **Policy CM 7.5**

Consider the least impactful solutions to improve street capacity first, by utilizing priorities outlined in the Scottsdale Transportation Master Plan.



Site design features, such as ground floor retail, provide access to pedestrians, bicyclists, and transit users.